

CHAPTER 2

Project Description

A. Project Setting

The proposed project is located in the City of Long Beach, which is approximately 22 miles south of downtown Los Angeles on San Pedro Bay (see **Figure 2.1**). The City is home to the Port of Long Beach. Major employers, in addition to the Port, include Boeing, tourist attractions, a state university, a city college, the City of Long Beach, and Long Beach Unified School District (LBUSD). The City is approximately 52 square miles and services a population of approximately 462,000 people.¹

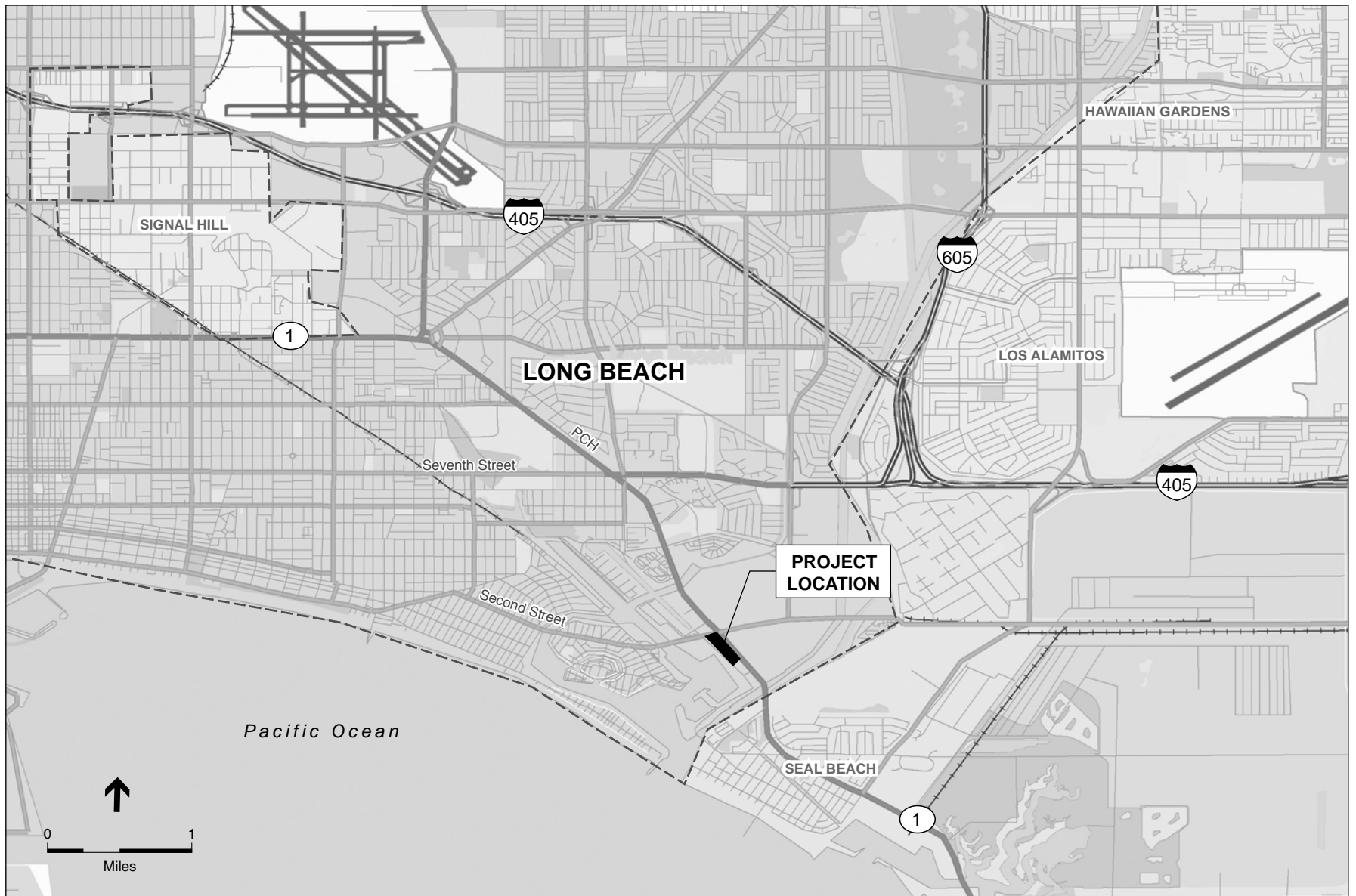
The proposed project site is located in between the San Gabriel River and the Los Cerritos Channel at the southwest corner of PCH and Second Street. The project site is roughly bounded by Second Street to the north, a retail center to the south, PCH to the east, and Marina Drive to the west (see **Figure 2.2**). The site is located approximately five miles east of downtown Long Beach and approximately two miles south of the I-405.

The project site is primarily occupied by the 250-room Seaport Marina Hotel. Commercial uses within the hotel include an Enterprise Car Rental, The Elks Club and a nightclub, which are located near the hotel lobby. Vehicular access is provided via an existing system of roadways with direct access from PCH, Second Street, and Marina Drive. Portions of the project site were used as both a former oil well site and service station. Although the service station has been removed and remediated, there is on-going groundwater monitoring; the former oil wells are required to be re-abandoned according to current state regulations; and any contaminated soil and groundwater associated with the abandoned oil wells (e.g., mud sumps) would be remediated during site excavation (see Section 3E. Hazards for additional information). In addition, a crude oil pipeline is located along the eastern boundary of the project site.

The project site is designated in the General Plan as Land Use District (LUD) No. 7 and zoned Planned Development (PD-1) (Southeast Area Development and Improvement Plan [SEADIP]), Subarea 17.²

¹ US Census Bureau, Summary File 1 (SF 1) and Summary File 3 (SF 3), 2000.

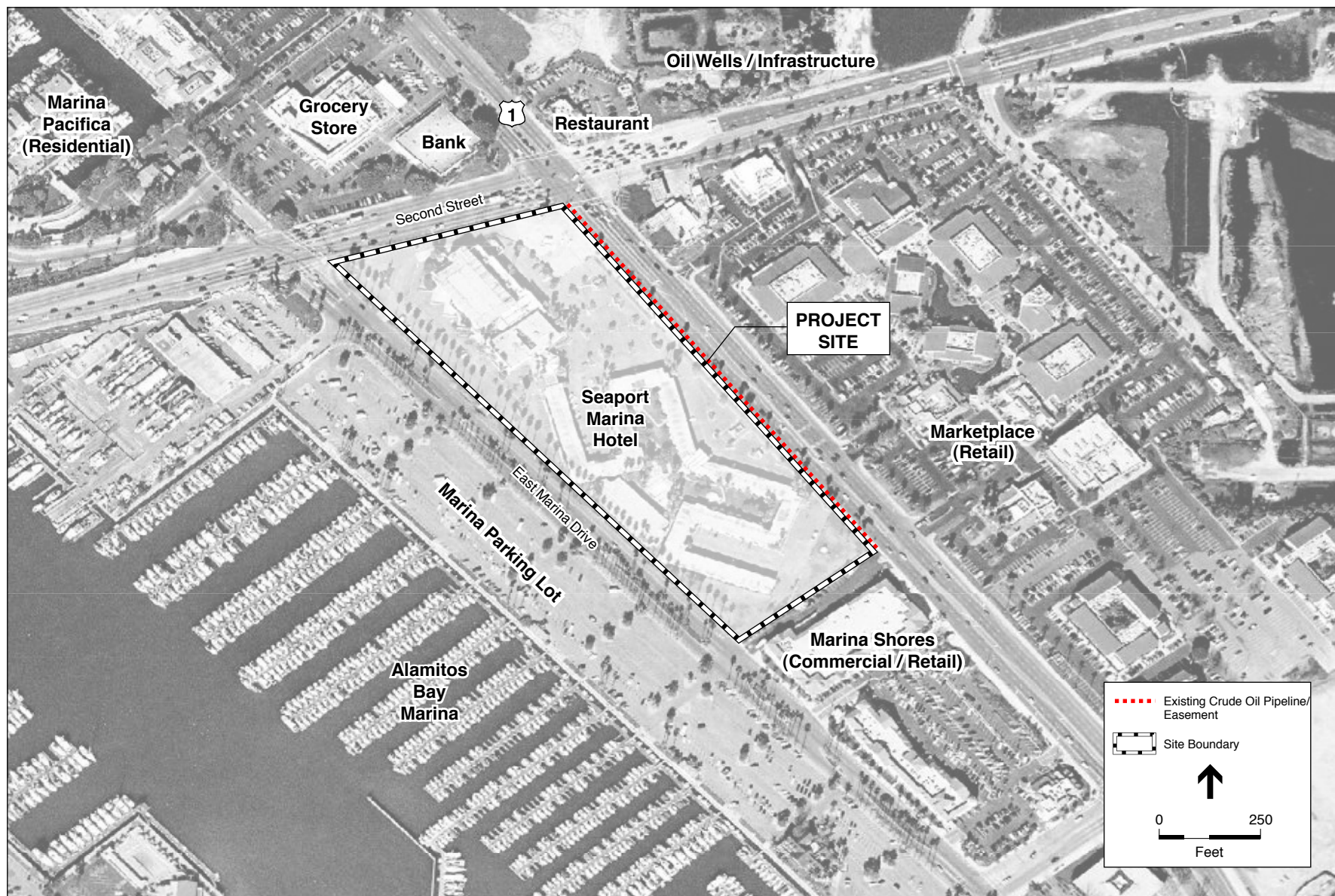
² City of Long Beach, Department of Planning and Building, *Land Use Element of the Long Beach General Plan*, revised and reprinted April 1997, page 169; City of Long Beach Department of Planning and Building, *Zoning Ordinance (Title 21 of the City of Long Beach Municipal Code)*.



SOURCE: StreetMap USA; ESA, 2006.

Long Beach Marina EIR . 204452

Figure 2.1
Site Vicinity



SOURCE: GlobeXplorer 02-01-2005; ESA 2005.

Long Beach Marina EIR . 204452

Figure 2.2
Site Location

PD-1 is a Planned Development (PD) area that may benefit from the formal recognition of unique or special land uses and the definition of special design policies and standards not otherwise possible under conventional zoning district regulations. The PD district allows a compatible mix of land uses, planned commercial areas and business parks, and encourages a variety of housing styles and densities.³

PD-1 has a total of 33 subareas, providing for a community of residential, business, and marine-related uses integrated by an extensive system of parks, open space, and trails. Subarea 17, within which the project site is located, is fully developed with commercial and service uses. In reviewing and approving site plans and tract maps for development of the areas within PD-1, the City is guided by the goals and policies of this PD district.⁴

The project site and much of the surrounding area is subject to the Local Coastal Program (LCP). The LCP is an adopted component of the City's General Plan. It is also a California Coastal Commission approved land development and land use plan.

As shown in Figure 2.2 and described below, the project site is located in an urbanized area with retail and commercial uses that are located along the major roadways bordering the site.

- North: Uses along Second Street include a one-story grocery store and bank. The Marina Pacifica Mall, which includes larger retail, restaurant and entertainment uses, is located north of the grocery store and bank. These uses are setback along PCH, and all have surface and some subterranean parking. The area to the northwest of the project site is Marina Pacifica, a private waterfront community consisting of attached residences. The residences are condominiums, ranging from three to five stories in height. Also to the northwest is the Long Beach Marina with a boat launch located south of the Marina Pacifica condominiums. The area northeast of the site consists of a fast food restaurant (northwest corner of Second Street and PCH), oil wells and the Los Cerritos wetlands.
- South: Adjacent to the project site along PCH is Marina Shores, a retail center with restaurants, a grocery store, services, and other retail. This center continues to the intersection of PCH and Studebaker Road. Beyond Studebaker Road, southeast of the project site, are more oil infrastructure facilities and a two-story office building, to the southwest, and the San Gabriel River.
- East: Land uses near the intersection of Second Street and PCH include a service station (southeast corner of Second and PCH). Across from the site on PCH, is The Marketplace, a one-story retail center that includes several restaurants, a grocery store, many small retail shops, and movie theaters. South of the retail center on the east side of PCH, are several one- and two-story office

³ City of Long Beach, Zoning Code, Chapter 21.37.

⁴ City of Long Beach, Department of Planning and Building, *Land Use Element of the Long Beach General Plan*, revised and reprinted April 1997, page 169.

buildings and the Los Cerritos wetlands. In addition, a crude oil pipeline and easement is located along the eastern boundary of the site (see Figure 2.1).

- West: Directly west of the project site (across Marina Drive) is the publicly-owned Alamitos Bay Marina. The parking lot for the Marina occupies most of the area west of the project site (approximately 1,177 parking spaces). Along Marina Drive are restaurants and some boat related retail.

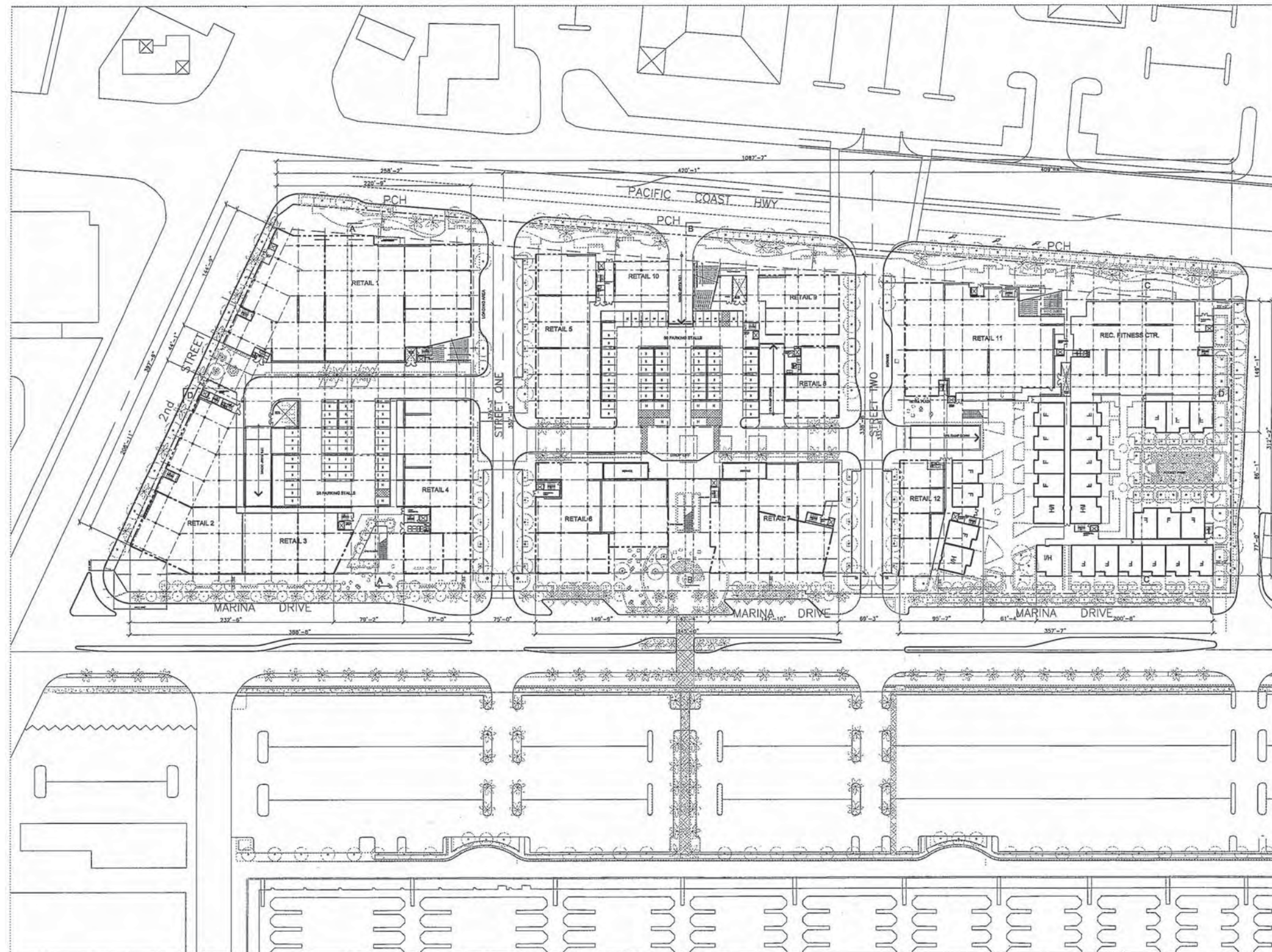
B. Project Characteristics

The project applicant, Seaport Marina, LLC, is seeking to redevelop the proposed project site with up to 170,000 square feet of retail development and 425 residential units in structures up to five stories in height (maximum 68 feet) (see **Figures 2.3** and **2.9**). The proposed project is a retail/residential development divided into three blocks as follows:

- Block A is the northern block on the site bordered by Second Street and would include approximately 80,000 square feet of street level retail space and 127 residential units. This block also includes three outdoor plazas, one along Second Street and two along Marina Drive;
- Block B is the central portion of the site and would include approximately 60,000 square feet of street level retail space and 144 residential units. This block includes a large outdoor plaza on the west side of the site along Marina Drive; and
- Block C is the southern most block, adjacent to Marina Shores, and would include 154 residential units, a 16,659 square foot recreational/fitness center for residents and approximately 30,000 square feet of street level retail space.

The project would be oriented toward Alamitos Bay Marina to allow for visitor and residential access and linkages to the marina and other area amenities. The project would be designed to be compatible with surrounding uses in terms of building height and size. The proposed project would provide approximately 20 percent of the site as open space. The proposed project would also include a bike/pedestrian pathway along Marina Drive, and would encourage pedestrian activity between the development and the marina.

Demolition of the existing on-site buildings (164,736 square foot Seaport Marina Hotel) would be required to allow for project construction. The proposed retail and residential components would be integrated as described above. **Table 2.1** provides a list of project components and a description of each.



SOURCE: Lennar, 2006.

— Long Beach Marina EIR . 204452
Figure 2.3
Conceptual Site Plan



SOURCE: Lennar, 2006.

Long Beach Marina EIR . 204452
Figure 2.4
Project Site Elevations
Block A (West/East)







SOURCE: Lennar, 2006.

Long Beach Marina EIR . 204452
Figure 2.7
Project Site Elevations
Block A (North/South)



SOURCE: Lennar, 2006.

Long Beach Marina EIR . 204452
Figure 2.8
Project Site Elevations
Block B (North/South)



SOURCE: Lennar, 2006.

Long Beach Marina EIR . 204452
Figure 2.9
 Project Site Elevations
 Block C (North/South)

**TABLE 2.1
PROJECT COMPONENTS**

Project Component	Description
Retail	<ul style="list-style-type: none"> Up to 170,000 square feet of street level retail uses.
Residential	<ul style="list-style-type: none"> 425 residential units.
Street improvements	<ul style="list-style-type: none"> Three access points from PCH. Right-of-way dedication on the west side of PCH. Traffic signal at primary entry site on PCH. Relocation of drive entrance into Market Place parking lot across PCH to line up with proposed primary entry. Improvements provide for transition lane at PCH and Second Street, two through lanes and deceleration/acceleration lane for project. Closure of existing driveways and repair of any existing damaged curb and sidewalk on the south side of Second Street. Reconstruction of Marina Drive southeast of Second Street to the southeast project property line. Includes two northwest bound lanes (11-foot outside lane, 11-foot inside lane), 16-foot islands with left turn lanes at entrance points of project on Marina Drive and Marina public parking. A minimum of six-foot island nose at all left turn pockets on Marina Drive. Includes an existing right turn lane from Marina Drive to eastbound Second Street. Includes a 14-foot southeast bound lane on Marina Drive.
Bike/pedestrian paths	<ul style="list-style-type: none"> Construction of 5-foot landscaped area, an off-street Class I bike trail, 6-foot pedestrian sidewalk on southwest side of Marina Drive from Second Street to Studebaker. Extends off-street bike trail and six-foot pedestrian sidewalk and crossing to Studebaker Road on the southwest side of Marina Drive.
On-site parking	<ul style="list-style-type: none"> Approximately 1,700 on-site parking spaces in PCH garage.
Off-site parking	<ul style="list-style-type: none"> Limited angled parking on east side and parallel parking on west side of Marina Drive Improvements to City-owned parking lot including re-alignment of drive aisles and landscaping.
Site demolition and debris removal	<ul style="list-style-type: none"> Continued monitoring of portion of the site formerly used as a service station; proper re-abandonment of former oil wells; and possible encounter with other previously unknown USTs. Grading. Fill removal and recompaction. Removal of existing structures (for example, hotel) and other property improvements. Required recycling of demolition material.
Project landscaping and open space	<ul style="list-style-type: none"> Parkway landscaping. Perimeter landscaping. On-site landscaping in both public and private open space areas. Off-site landscaping (Marina Drive and marina parking lot). A two-to-one tree replacement per City code, as necessary, for any trees removed from the public right-of-way.

TABLE 2.1
PROJECT COMPONENTS (CONT.)

Project Component	Description
Storm drain	<ul style="list-style-type: none"> Reconstruction of existing storm drain to redirect flows to an existing storm drain on southwest side of property. A new storm drain would be constructed from PCH across the project site to Marina Drive to replace existing 36-inch storm drain crossing property.
Water quality improvements	<ul style="list-style-type: none"> Treatment Best Management Practices (BMPs) such as bio-retention for treatment of runoff from the site.

Retail Buildings

The proposed project includes up to 170,000 square feet of retail located at ground level. The retail spaces would relate architecturally to the elements of the residential units, thereby creating a consistent visual character within the mixed-use development.

Residential Units

The 425 residential units would be located throughout the project site above the retail spaces and oriented to maximize views. The residential units would be designed with a mixture of multi-family units including lofts, townhomes, and flats. The integrated retail and residential buildings would be up to five stories with a maximum building height of 68 feet. Figures 2.5 through 2.9 show the proposed building elevations.

Access, Parking, and Circulation

Access to the site would be provided at three locations along PCH and two locations along Marina Drive. Along PCH access would be provided by a new private street between Blocks A and B, which would continue through to Marina Drive; a right-in, right-out entry leading to at grade parking on Block B and a new signalized primary entry from PCH onto a new private street leading to retail and residential parking garages between Blocks B and C, which would also continue to Marina Drive (see Figure 2.3).

Additional pedestrian access would be provided along Second Street and Marina Drive, including a pedestrian crossing at the plaza leading to Alamitos Bay Marina. The proposed project would establish a Class 1 bike trail connection and maintain the existing pedestrian crossing at Second Street and Marina Drive. The proposed project would also include a transition lane at Second Street, two through lanes, and a deceleration/acceleration lane.

The proposed project includes improvements to the streetscape along PCH at select locations adjacent to the project site to accommodate a six-foot sidewalk. The project

also proposes improvements along the southwest and southeast sides of Marina Drive to Studebaker for an off-street bike trail and sidewalk. In addition, the proposed project includes the closure of existing driveways and repair of any existing damaged curbs and sidewalks adjacent to the project site (south side of Second Street).

There would be approximately 1,700 total on-site parking spaces including residential parking, and shared retail and guest parking. The project proposes improvements to Marina Drive (between Second Street and Studebaker) to allow on-street angled (east side) and parallel parking (west side).

Related Site Improvements

Other proposed site improvements would include construction of refuse enclosures, security lighting, and signage. The building materials and consistent integrated architecture design would be visible from the public rights-of-way and would address the guidelines outlined in PD-1.

Landscaping and Open Space

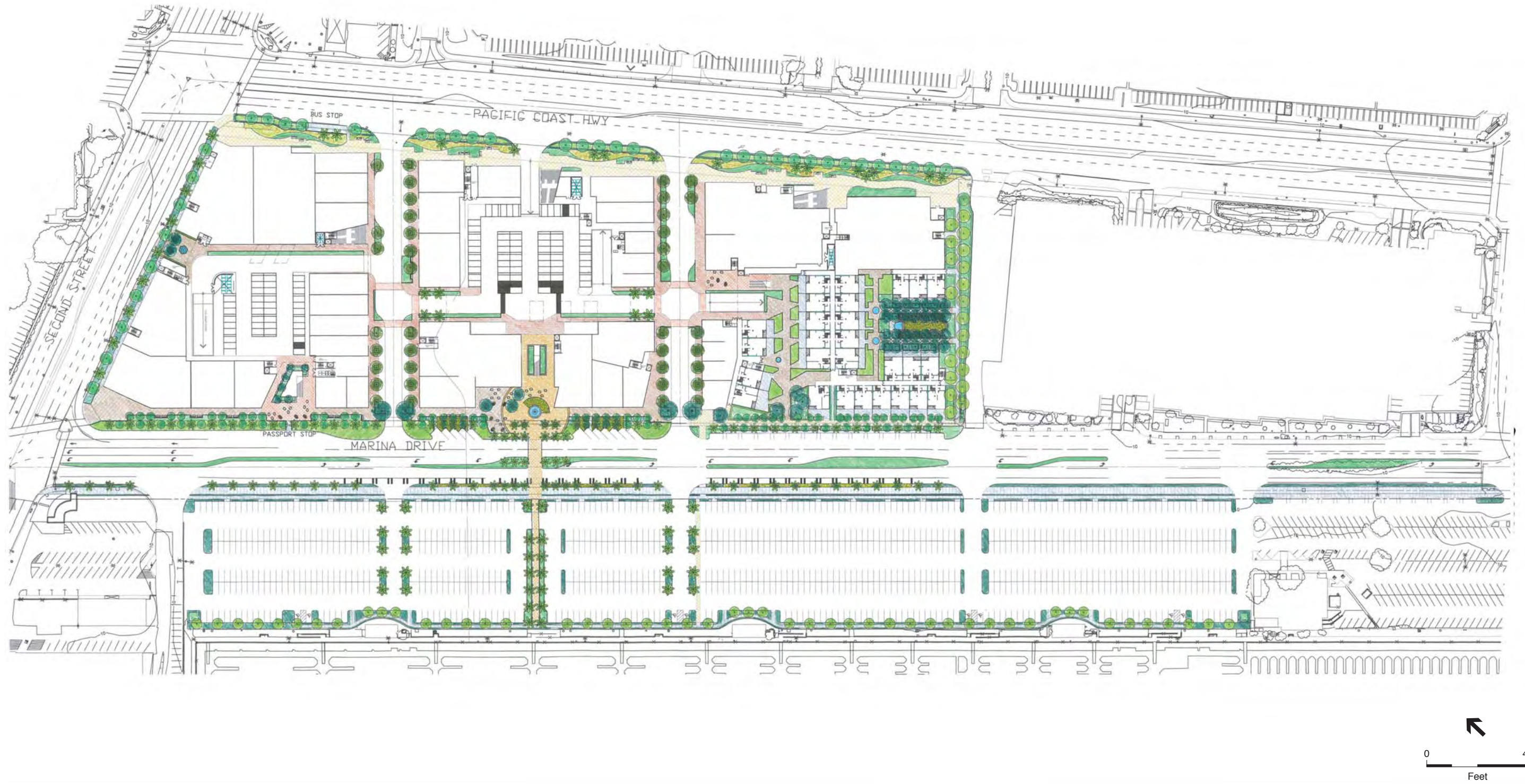
The landscaping plan for the project is presented in **Figure 2.10**. As illustrated, project landscaping would include perimeter planting, internal streetscapes, plazas and patios. The proposed project would include approximately 20 percent of the project site as open space.

Landscape improvements along Marina Drive would consist of maintaining the existing identity with the Mexican fan palms lining both sides of the street, and would also include shrub and groundcover under-story planting. Landscaping would consist of native California species and varieties.

In addition, the southwest side of Marina Drive would include an off-street Class I bike trail and a six-foot pedestrian path. The parkway planting along Second Street would include a mix of canopy trees and the Mexican fan palms that extend out from Marina Drive.

The wider edge along PCH would allow for a landscape buffer from vehicular traffic. Adjacent to the curb is the parkway that would include small canopy trees planted with shrubs in accordance with Caltrans guidelines. Additional planting would be added around the existing Mexican fan palms to create a garden edge to the sidewalk. The southeast perimeter adjacent to Marina Shores would include trees and large shrubs in order to provide privacy for proposed project residents and screen unwanted views of the service and delivery area of the existing retail development.

The internal private streets would include flowering trees, lower shrubs and groundcover. The plan also includes tree grates to be used in areas adjacent to parallel parking spaces.



SOURCE: Lennar, 2006.

Long Beach Marina EIR . 204452
Figure 2.10
 Proposed Landscape Plan

Mexican fan palms would be included into the main plaza on Marina Drive (Block B) from the parkway in planters. This area would also include a water feature that would be viewed from the street and by residents above. The entry plaza off of Second Street would serve as another entry point into the site with a small grouping of trees clustered around a water feature. Residential courtyards on the ground level would include private gardens that frame the garden court.

Off-site landscaping improvements would consist of a tree lined pedestrian promenade connecting the main plaza to the waterfront. The landscaping proposed in the marina parking lot would include new trees, shrubs and groundcover including California native species and varieties. The plan would also feature another walkway on the same level as the parking lot that runs parallel to the waterfront.

All planted areas would be irrigated according to plant type and environmental exposure by an automatically controlled, electrically activated underground piped irrigation system to conserve water and minimize erosion. All landscaping plans and irrigation systems would conform to City Zoning Code requirements for on-site landscaping and street trees.

Infrastructure

Development of the proposed project would include the provision of necessary infrastructure, including drainage, sewage disposal, water, solid waste, electricity, natural gas, and telecommunications. The project infrastructure components would require improvements to, and connection with, off-site and on-site infrastructure systems.

These systems, consisting of water, electricity, natural gas, telephone, and cable television / telecommunication lines, sewerage, storm drains, and street improvements, would be constructed on- and off-site and would be fully provided and maintained by the property owners (on-site facilities), municipal agencies, or utility service providers. See Table 2.1 for a list of infrastructure improvements.

A backbone infrastructure plan has been developed to serve the proposed uses. Infrastructure plans and connections to off-site utilities are further described and assessed in Section 3J. Public Services and Utilities.

Water and Sewer Services

The on-site water system would be considered private property and would be maintained by the applicant. The on-site sewer system would also be maintained by the applicant. Gravity sewer lines in public streets or Long Beach Water Department (LBWD) easements would be designed to their standards.

Storm Drain System

A comprehensive surface drainage/storm drain system has been developed to collect and convey runoff on the project site. This system includes reconstruction of an existing storm drain to redirect flows to an existing storm drain on the southeast side of the property. A new storm drain would be constructed from PCH across the project site to Marina Drive to replace the existing 36-inch storm drain crossing the property (**Figure 2.11**).

Lighting

Security lighting is proposed throughout the project site and would consist of energy-efficient luminaires. To control nighttime lighting spillover and glare, parking lot lighting would be designed with a reflector system to restrict light to lower portion of the lighted area (for example, direct light down instead of into the night sky) and turned off after business hours with the exception of security lights.

Construction Process and Timeline

It is anticipated that project construction would be completed in one phase over approximately 22-months. Construction of the proposed project would involve demolition (two months); clearance and excavation (four months), and building construction (16-months). The anticipated construction would begin with Block A, continue with Block B and finish with Block C.

C. Project Objectives

As the population of Long Beach and the remainder of Southern California increases, additional housing and services, including retail opportunities are in demand. The development of the proposed Seaport Marina project would provide the residents of Long Beach and the surrounding area with expanded retail options and the region with new housing opportunities. The project site is located in the southeast gateway to the City. In addition to providing increased visibility of the waterfront for visitors and residents, the development of the proposed project would provide an additional source of revenue for the City. The City developed the following guiding principles in January 2005:

Principle 1: The City will work with the applicant to create a vibrant retail center on the site. The City acknowledges that as part of this project, housing may be permitted, provided, however, that the housing is developed concurrently with the retail center, and that a truly integrated mixed-use project results.

Principle 2: The project should strive to meet public open space objectives currently set forth in SEADIP and consistent with the spirit and intent of the Parks, Recreation, and Marine Department's 2003 Strategic Plan.

Principle 3: The City will work with the applicant to ensure an aesthetically attractive, high quality design that reflects the property's unique orientation near a wetlands open space resource and adjacent to an active marina.

Principle 4: The City will work with the applicant to ensure that appropriate mitigation measures are adopted to ameliorate traffic conditions near and around the project site.

Principle 5: The project should strive to provide a high level of accessibility to and through the site. A well-defined circulation pattern will ensure a high-quality pedestrian environment, efficient vehicular access, and access to mass transit.

The following project objectives are based upon these guiding principles:

- Create a mixed-use project that includes a vibrant retail center on the site.
- Create an aesthetically attractive, high quality design that reflects the property's unique orientation adjacent to an active marina.
- Provide amenities that encourage and promote public access to the marina.
- Provide a high level of accessibility to and through the site to ensure a high-quality pedestrian environment, efficient vehicular access, and access to mass transit.
- Provide an economical reuse of the project site while minimizing adverse impacts to surrounding properties.
- Design and implement comprehensive site development standards that minimize adverse impacts to the environment.
- Enhance the economic vitality of the City and provide property tax, sales tax, and other revenue opportunities.

D. Discretionary Actions

The purpose of this EIR is to analyze the proposed development and activities further described and analyzed in Chapter 3. It is intended to apply to all listed project approvals as well as to any other approvals necessary or desirable to implement the project.

This EIR is intended to inform decision makers and the public of the environmental effects of implementing the proposed project and of the mitigation measures or available alternatives that could reduce or avoid significant impacts.

This EIR analyzes and documents the impacts of the proposed project and all discretionary and ministerial actions associated with the project. The City, as Lead Agency, will use this EIR in assessing the effects of the City actions detailed below. This EIR includes discussion of state and local governmental discretionary approvals that may be required to construct or implement the proposed project, whether or not they are explicitly listed below. The known discretionary approvals required to implement the proposed project include the following:

- General Plan/Local Coastal Program Amendment: The Local Coastal Program does not allow residential uses on the project site. Therefore, since the proposed project includes residential units, approval of the project must include an amendment to the Local Coastal Program. In addition, since the Local Coastal Program is adopted as a component of the General Plan, approval of the project also requires an amendment to the General Plan.
- SEADIP (PD-1) Amendment: The project site is currently located in Subarea 17 of PD-1, which allows for commercial uses only. As a result, approval of the project requires an amendment to PD-1.
- Site Plan Review: Due to the size of the proposed project, Site Plan Review approval is required. The Site Plan Review process is established to meet certain community goals which are, among others, to ensure that the highest quality of land planning and design are incorporated into development projects, to ensure that new projects are compatible with existing neighborhoods in terms of scale, style and construction materials, and to ensure the maintenance, restoration, enhancement and protection of the environment.

In addition to the review of the overall site planning, design, and neighborhood compatibility, the Site Plan Review process provides an opportunity to review the project consistency with the Guiding Principles jointly developed by City staff and the project applicant.

- Tentative Subdivision Map: A Tentative Subdivision Map would be required in order to divide the project site into multiple parcels and to allow for the individual sale of the units.
- Standards Variances: The project, as proposed, provides less than the required usable open space and less than the required setback along Second Street. Therefore, approval of the proposed project would also require approval of Standards Variance Requests for these items.
- Local Coastal Development Permit: The proposed project requires approval of a Local Coastal Development Permit since the project site is located in the Coastal Zone and requires additional discretionary approval from the City.

E. Cumulative Development

Cumulative impacts refer to the combined effect of project impacts with the impacts of other past, present and reasonably foreseeable future projects. Both CEQA and the *CEQA Guidelines* require that cumulative impacts be analyzed in an EIR. As set forth in the *CEQA Guidelines* Section 15130(b), *“the discussion of cumulative impacts shall reflect the severity of the impacts, and their likelihood of occurrence, the discussion need not be as detailed as the discussion of environmental impacts attributable to the project alone.”*

According to Section 15355 of the *CEQA Guidelines*,

“Cumulative impacts’ refer to two or more individual effects which, when considered together, are considerable or which compound or increase other environmental impacts.

- a) The individual effects may be changes resulting from a single project or a number of separate projects.*
- b) The cumulative impact from several projects is the change in the environment which results from the incremental impact of the project when added to other closely related past, present, and reasonably foreseeable probable future projects. Cumulative impacts can result from individually minor but collectively significant projects taking place over a period of time.”*

Therefore, the cumulative discussion in this EIR focuses on whether the impacts of the proposed project are cumulatively considerable within the context of impacts caused by other past, present, or future projects. Cumulative impact discussions for each issue area are provided in the technical analysis contained within Chapter 3, Sections A through M. **Table 2.2**, Cumulative Project List, provides a list of the cumulative projects that are considered in this cumulative environmental analysis, and comprise development projects planned in the proposed project area.

**TABLE 2.2
CUMULATIVE PROJECT LIST**

Name	Status	Comments
1. Boeing Seal Beach	Under construction	344,124-square foot business park; 2,281,691-square foot business park; and 198,020-square foot retail and 120-room hotel on 64.84-acre site
2. Home Depot	In process	157,529-square foot building (104,866-square foot store, 34,642-square foot garden center, 8,050-square foot restaurant, 12,000-square foot retail) on 16.70-acre site
3. Marina Shores East	On hold	73,000-square foot building (43,000-square foot supermarket, 30,000-square foot retail) on 7.05-acre site

SOURCE: City of Long Beach, 2005.

The location of these projects is included in **Figure 2.12**.

Cumulative projects are those projects that are in various stages of the application and approval process. The cumulative project list was compiled using information provided by the City of Long Beach.



SOURCE: ESA, 2006; GlobeXplorer, 2006.

Long Beach Marina EIR . 204452
Figure 2.12
 Location of Cumulative Projects

Cumulative impacts analyzed in this EIR (impacts from related projects in conjunction with the proposed project) would likely represent a conservative scenario that would over-estimate impacts for the following reasons:

- It is possible that some of the future projects will not be constructed or opened until after the proposed project has been built and occupied.
- Impact projections for future projects would likely be, or have been, subject to unspecified mitigation measures, which would reduce potential impacts.
- Many future projects are expressed in terms of gross square footage or are conceptual plans such as master plans that assume complete development; in reality, such projects may be smaller (i.e., the net new development) because of the demolition or removal of existing land uses resulting from development of the related project.

The potential for cumulative impacts to occur from the proposed project could include, but is not limited to:

- Within local context: development of the proposed project in conjunction with other project in the nearby area could result in locally significant impacts (e.g., construction related impacts).
- Within the regional context: development of the proposed project in conjunction with other projects could result in regionally significant impacts (e.g., air quality).